

METHOD OF WORKING PLAN [MOWP]

AERODROME NADI INTERNATIONAL AIRPORT, NADI (NFFN)

FA – GATES 1-3 & 2 CODE E OFFGATE STANDS

PROJECT DESCRIPTION Expansion of Taxiway to enable reconfiguration of Gates 1-3 to the north and to make space for the relocation of the airside walkway. This will require modifications to the apron area to cater for fuelling points and increase PCN values (structural capacity) of the pavements, as well as the design of two new Code E offgate stands on the green field land between the FA hangar and terminal building.

REFERENCE NUMBER 12506576

DATES

Approval of MOWP: 5th February 2020

Mobilization of Works: 10th February 2020

Commencement of works : 27th March 2020

Completion of Work: 20th December 2021

Expiry of MOWP: 20th December 2021

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1 WORKS INFORMATION

Background

Fiji Airports Limited, has identified the need for more apron hardstand aircraft parking area at Nadi International Airport, which is in alignment with the Nadi International Airport Masterplan, draft dated November 2018.

As the first stage of Apron and Taxiway expansions, Fiji Airports now wish to progress the construction of a new concrete hardstand area north of Gates 1-3. This also involves the widening / realignment of the taxiway centreline.

Works have been divided into two Separable Portions. The following MOWP is applicable to Separable Portion 1 (Main Works Package) only.

Description of Work

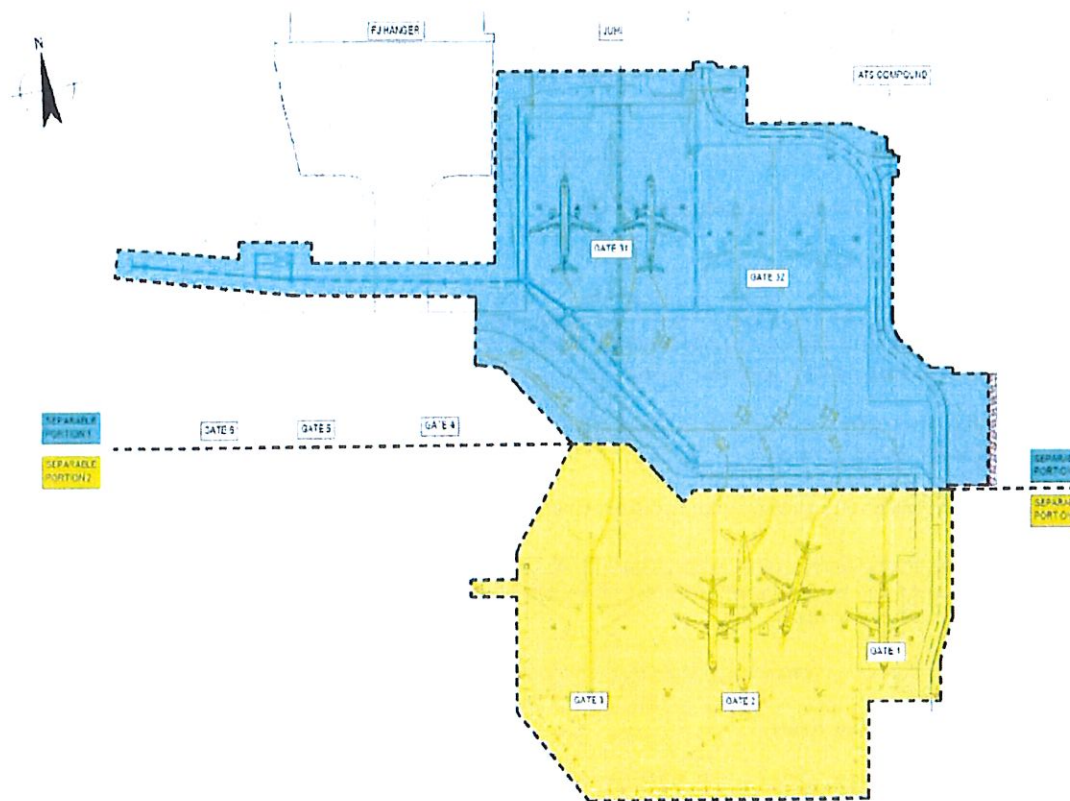
This staged development comprises the following construction items, but not necessarily limited to:

Separable Portion 1 – Offgates 2 Code E / 4 Code C stands:

- Removal of existing slot drain located between the concrete and asphalt shoulder pavements
- Construction of new concrete hardstand area to widen taxiway and construct two new MARS stands
- Reconfigure alignment of taxiway centreline
- Install new AGL taxiway centreline and edge lighting and blank out old redundant AGL pots
- Installation of new floodlights and associated underground services
- Installation of stormwater drainage network including heavy duty slot drains and heavy duty manholes
- Complete the construction of the airside road between the JUHL compound and Gate 1
- Install a new aircraft bailout facility and decommission and remove existing facility
- Installation of new fuel hydrants and fuel pipelines for the new gates
- Install new stormwater pipe across / underneath the Fiji Airways taxiway
- Install new Oil separator devices west of the Fiji Airways apron
- Install new paint markings in accordance with international standards

Separable Portion 2 (For Information only) Gates 1 to 3:

- Repair and rehabilitation of concrete slabs on Gates 1-3
- Expansion of the hardstand area east of Gate 1
- Replacement of concrete slabs on all gates
- Reconfiguration of paint markings for gates
- Installation of new fuel hydrants and fuel pipelines for reconfigured gates
- Installation of new floodlights and associated underground services



1.2.4 Construction Traffic

It is proposed that construction traffic will enter and exit airside via the nominated security gate and use only the nominated construction access route from this security gate to the FA agreed Contractor's laydown area(s). Refer Appendix B drawing 12506576-C194 which shows possible laydown areas. Alternative laydown sites and access routes can be consulted with FA should they be required.

Movement of vehicles, plant and equipment must be confined to these routes in order to minimise tracking of dirt and debris onto aircraft movement area pavements and to prevent damage to airport lighting.

The Civil Contractor is required to keep all pavements used or traversed during the works clean to the satisfaction of the SO. The Contractor shall use mechanical sweepers or suction trucks for the purpose of keeping the pavement surfaces clean and FOD free.

In the event any of the contractor vehicles damaging any facility, pavement, perimeter road or the environment during the work, the Contractor shall replace or repair the damage immediately.

Access to Unrestricted Work Zones

Access from the agreed Contractors Laydown area(s) to adjacent unrestricted work zones shall not require SO escort, unless operational pavements and/or clearance zones need to be traversed.

Access to Operational Pavements

Access to the work zones in operational pavements, or in close proximity to, that require restricted aircraft movements notified by NOTAM, for Contractor's personnel, plant and equipment will be under strict Safety Officer (SO) escort.

Specific access routes to and from the works area will be directed by the Safety Officer.

Commencement Date and Execution of Works

1.3.1 Timing

A NOTAM will be in place for all works. The actual date and time of commencement will be advised by NOTAM, and is to be issued by FA 48 hours before work commences or as soon as possible when the 48 hours' notice cannot be achieved.

The Contractor shall give two clear working days' notice (Monday - Saturday) prior to commencement of works.

Works will be arranged so as not to disrupt scheduled aircraft movements, emergency flights.

When unavoidable, works within operational clearance zones or airside pavements shall be consulted and work programs agreed with FA and stakeholders at least one month in advance of works being carried out.

Works shall cease whenever directed by the Works Safety Officer (SO).

1.3.2 Working Hours

Hours of working will be as follows (based on current airline schedules, may change as schedules alter)

Day Shift hours of working will be as follows: Monday to Saturday – 7AM to 7PM.

Night Shift works will be as follows:

- Monday 00:00 to 05:30 / 22:00 to 24:00
- Tuesday – 00:00 to 05:00
- Wednesday 22:00 to 24:00
- Thursday 00:00 to 04:00 / 22:30 to 24:00
- Friday 00:00 to 05:00 / 22:00 to 24:00
- Saturday 00:00 to 04:00 / 22:30 to 24:00
- Sunday 00:00 to 05:30 / 22:00 to 24:00

The above times may change due to Delays in Aircraft departure and access to site will only be given once the SO has given the all clear.

These works shall be consulted and work programs agreed with FA and stakeholders at least two weeks in advance of the works being carried out.

No work is permitted on Sundays (Saturday 7PM to Sunday 7PM)

1.3.4 Programme of Works

Mobilization period: 20 Days

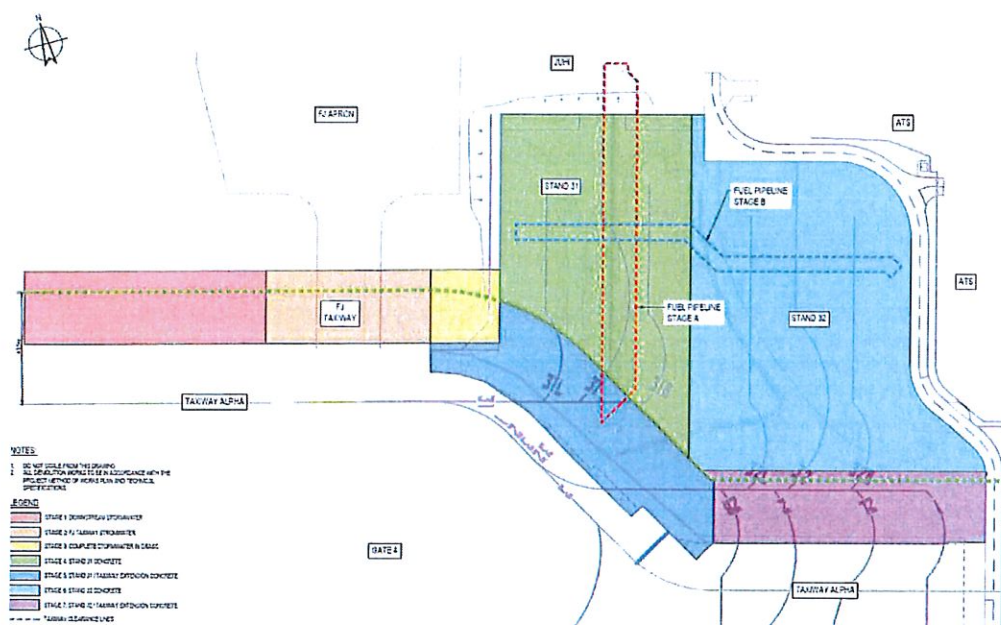
The Civil works period: 23 Months

Separable Portion 1 - 682 Calendar days from the date of mobilisation and includes for 117 calendar days of inclement weather.

Separable Portion 2 – to commence following completion of SP1 and subject to formal endorsement of the Fiji Airport's board.

1.3.5 Sequence of Work

The following sequence of works to be undertaken in order to complete SP1 works:



SP1: Code E Offgate stands and taxiway extension: 10th February 2020 – 20th December 2021

Phasing of fuel line configuration works to be aligned and concurrent with Phases A through D:

Stage A – Main Fuel Pipeline Construction and Decommissioning: 27th March 2020 – 2nd May 2020

The majority of the works will be undertaken with no restriction to Contractor. Towards the southern end of the work zone, works to be restricted to daytime vacate or night time works.

Stage B – Hydrant Fuel Pipeline Construction: 6th May 2020 – 19th June 2020

All works undertaken with no restriction to the Contractor.

Stage 1 – Downstream stormwater works: 27th March 2020 – 24th April 2020

The works breach taxiway-to-construction zone separation for Code E aircraft. These works are to be undertaken during curfew / night time shifts when no aircraft are operating.

Stage 2 – Fiji Airways Taxiway Culvert: 25th April 2020 – 22nd June 2020

The works breach taxiway-to-construction zone separation for Code E aircraft. Six 12-hour closures are required to undertake the culvert construction. The six closure periods are scheduled for:

- Saturday 25th April from 9pm

- Thursday 07th May from 9pm
- Saturday 09th May from 9pm (following closure, aircraft restricted to east side of taxiway)
- Thursday 18th June from 9pm
- Friday 19th June from 9pm
- Monday 22nd June from 9pm (following closure, aircraft restricted to west side of taxiway)

Stage 3 – Completion of Taxiway Strip Stormwater works: 16th May 2020 – 23rd July 2020

The works breach taxiway-to-construction zone separation for Code E aircraft. These works are to be undertaken during curfew / night time shifts when no aircraft are operating.

Stage 4 – Gate 31 construction: 23rd March 2020 – 27th April 2021

All works undertaken with no restriction to the Contractor. Inclusive of concrete hardstand construction, fuel pipeline excavation and backfilling, hydrant block outs, slot drain and stormwater network construction.

Stage 5 – Gate 31 & Taxiway extension construction: 18th July 2020 – 2nd February 2021

The works breach taxiway-to-construction zone separation for Code E aircraft. These works are to be undertaken during curfew / night time shifts when no aircraft are operating. Concrete hardstand pavement construction; existing slot drain removal; new slot drain construction, stormwater pipe and heavy duty manhole installation; duct trench on existing taxiway concrete pavement.

Stage 6 – Gate 32 construction: 20th June 2020 – 27th August 2021

All works undertaken with no restriction to the Contractor. Inclusive of concrete hardstand construction, hydrant block outs, slot drain and stormwater network construction.

Stage 7 – Gate 32 & Taxiway extension construction: 10th May 2021 – 20th December 2021

Concrete hardstand pavement construction; existing slot drain removal; new slot drain construction; duct trench on existing taxiway concrete pavement. This will restrict the hours in which these works are carried out, due to the conflict with the minimum taxiway separation to existing, as well as potentially disrupting access to gates 1 and 2. This will require consideration by FA and contractor to ensure the airport can remain functional while the taxiway extension is constructed.

Once approval is given by FA to proceed with the work, only the machines and equipment required for the civil work shall be permitted to enter the work zone whilst the others remain landside.

At no time shall unnecessary vehicles be permitted airside unless specifically authorised by the SO.

2 SAFETY

2.1 Method of Working Plan

Works shall be done in strict accordance with this Method of Working Plan (MOWP)

2.1.1 Safety of Aircraft Operations at All Times

The Contractor shall ensure that the construction area is returned to service within 90 mins of being notified by the SO to stop all work for whatever reason.

The Contractor shall not breach the 45 metre clearance from taxiway centreline at any time during unrestricted operations during the Contract.

2.1.2 Daily Prior Works Notification.

The Works Safety Officer (SO) shall be notified by the Contractor at least 12 hours prior to shift starting if civil works (and hence access) will not be required the following day.

3. RESTRICTIONS TO AIRCRAFT OPERATIONS

The following restrictions are anticipated for Aircraft operations:

Fuel Stage 1 and Fuel Stage 2: No Restrictions

Stage 1: No Restrictions

Stage 2: Restrictions as follows:

- Six 12-hour closures.
- Following third closure, aircraft restricted to east side of taxiway for a minimum period of 21 days.
- Following sixth closure, aircraft restricted to west side of taxiway for a minimum period of 21 days.

Stage 3: No Restrictions

Stage 4: No Restrictions

Stage 5: No Major Restriction – Aircraft to be tugged to location clear of work zone prior to start-up

Stage 6: No Restrictions

Stage 7: No Major Restriction – Aircraft to be tugged to location clear of work zone prior to start-up

In the case of unforeseen conditions related to Weather or emergencies being forecasted before the construction shift commences, the works will be delayed or cancelled accordingly, to facilitate continued aircraft operations.

FA will publish aeronautical Information pertaining to the Nadi aerodrome operating status via NOTAM's. Any variations to that advised above will be notified via NOTAM.

4. RESTRICTIONS TO THE WORKS

General

The Contractor shall comply with the requirements of CAAF Standard Document SD 139-05 Operational Safety during Works on Aerodromes, this MOWP and the provision of the specifications for these works (Refer Appendix A).

The Contractor's site representative shall contact the SO at the start of each working shift to ascertain the status for the proposed work with respect to the operational requirements of the aerodrome.

All plant, equipment and materials shall be secured at all times during the work (on site and at laydown area) so that it is not be able to be wind borne. Plant and materials will be stored in such a manner to ensure that they do not infringe the minimum clearance distance for the existing taxiway and apron.

All lighting erected and operated by the Contractor shall comply with the Civil Aviation Authority Fiji requirements.

In the case unforeseen conditions related to Weather or emergencies being forecasted before the construction shift commences, the works will be delayed or cancelled accordingly.

All vehicles and plant operating on or near aircraft operational areas shall display a flashing orange light when vehicles are moving to and from the areas of work.

Driving

A speed limit of 30kph applies to all vehicles on the general airport movement area and 10kph when within 15 metres of aircraft. Breaches of these speed limits will result in refusal of access to the airside for the driver involved for the remainder of the works.

All vehicle drivers requiring access onto airside shall undertake safety awareness training delivered by FA and shall be escorted at all times by a SO whilst on the manoeuvring area.

Any non-compliance with respect to this plan shall see immediate removal of the vehicle and driver from the project. It is mandatory that all vehicles operating on the airside shall have third party and public liability insurance cover of not less than \$1M FJD.

Drivers must ensure that trucks are correctly loaded and where necessary covered prior to entering and leaving site to prevent any spillage. In the event of spillage the truck driver must immediately notify the project manager / safety officer who will oversee the cleaning of spilled materials by the Contractor.

All vehicular movements on airside shall be confined to the work areas and access routes indicated on drawing C431.

Pavement Cleanliness – FOD Protection

The Contractor is required to keep all aircraft pavements used or crossed during the works clean to the satisfaction of the SO.

No motor vehicle shall leave or enter the site laden with any materials unless the vehicle is loaded and/or covered in a manner that shall prevent the discharge or dropping of any materials.

The Contractor shall ensure that the wheels, tracks and body of all construction plant leaving the site are free from dirt or any other material, which may discharge or drop. Tracked plant or equipment shall not be used on any existing or new pavement surfaces.

Vehicles or plant not in use shall be parked and immobilised outside the works area or as nominated by the SO.

If the Contractor fails to keep these areas clean, the SO may arrange cleaning and the cost thereof shall be a debt due and payable by the Contractor to FA.

All damages to the existing surfaces tracked by vehicles of the Contractor shall be repaired to its original status by the Contractor.

Airport Security and Airside Safety Requirements

Every person desiring to enter the Site shall comply with all safety and security regulations relating to site and with all conditions relating to entry to and behaviour on the Site as contained in the Fiji Airports' Operational Specification.

Marking the Unserviceable Area

Markers will comprise white reflective banded orange cones or white banded red marker boards to define the limits of the work zone (if required).

After Completion of Work

Prior to operational handover, the works will be inspected and accepted for operational use and safety.

NOTAM

Works in Progress (WIP) will be arranged to minimise restrictions to schedule aircraft movements. The SO shall be responsible for coordinating the issue of NOTAMs relating to the Project and in accordance with paragraph 1.3 above. The NOTAM shall clearly define the dates and times when works will be in progress, what obstacles will be involved and procedures for clearing these obstacles prior to aircraft movement.

Denial of Access to the Works

During declared airport emergencies and/or poor visibility conditions, the Contractor may be either be refused entry to the Site or removed from the airside. Due to the unforeseen nature of these conditions, it may not be possible to give prior warning to the Contractor. In such circumstances, the

Contractor shall advise the Contract Administrator on a daily basis of any claim for cost or extensions of time.

Inspection Checklist & Operational Sign Off

An inspection checklist will be used during the works to ensure the airfield is left in a suitable condition for operational activities. An example checklist is provided in Appendix D.

Emergency and Adverse Weather

Under certain emergency situations or in the event of adverse weather conditions, the Contractor may be denied access to the airside of the airport and to the work areas. This will only apply prior to the scheduled commencement of the daily works.

In case of aircraft emergencies, the Contractor will comply with all SO Instructions for ceasing operations and removing plant and personnel from the immediate location.

In extreme adverse weather, the SO has the authority to stop the work where personnel or operational safety is considered at risk. Work will resume when those conditions abate but at the discretion of the FA.

In such an emergency situation the Contractor shall, if time permits, reinstate the works to a standard required at the close of each shift as follows:

All Materials, Vehicles, Personnel and Equipment shall be removed from the worksite as required under the operational requirements in the specification. This shall be as a minimum that no plant or materials shall intrude into any transitional surface.

The access road shall be swept and left clean of any dirt or loose debris that may cover markings, or reduce the smoothness or skid resistance of the surface or cause FOD tracking issues.

All surfaces shall be reinstated to safe condition for their use.

Complete an inspection of the site with the SO in accordance with the Inspection Checklist.

FA will provide a Safety Officer (SO) who will have complete authority to direct the Contractor on Aerodrome Operational Requirements.

All staff involved in the works must undergo a site specific briefing in relation to works safety and security requirements prior to commencing on site. FA will deliver the first briefing and thereafter the Contractor will be responsible for briefing new workers on safety and security requirements.

The Contractor shall comply with the requirements of the Contract Documents produced for this project and this MOWP. The Contractor's site representative shall contact the SO at least one hour prior to the start of each working period to ascertain the status for the proposed work with respect to the operational requirements of the aerodrome.

Smoking is not permitted on airside, or within any FA Airport buildings. A designated smoking area can be designated by FA outside the perimeter fence when requested to by the Contractor.

Upon completion of each work period and before each aircraft operation, the works areas are to be made serviceable to the satisfaction of the SO.

Personnel, Equipment, Plant and Materials

The nominated personnel for key roles are specified in Section 5: Administration.

All personnel involved on this job shall be bound by any instructions issued by the Safety Officer either verbally and/or written. The Safety Officer may refuse access to persons likely, in his opinion, to compromise aircraft safety on the aerodrome.

Personnel are to be confined to the defined work area and access routes at all times.

All staff are to wear high visibility jackets and safety boots or safety shoes whilst working on the airport. Staff must also wear hand gloves, dust protectors, safety glasses and ear muffs (as required through Contractors Health and Safety requirements).

The Contractors access will be limited to the work areas as shown on the Access Layout plan in Appendix B.

All construction work will be required to withdraw personnel and equipment from the construction area in the event of an emergency.

Only equipment, plant and materials that are required for daily construction activities shall be permitted Airside.

Aircraft Operators

Not applicable.

Works Limits

All works and the Contractors Constructor's plant, equipment, materials and personnel shall be confined to the areas established by the Contractor, in coordination with the SO and as shown in drawing C431.

The Contractor is to define the limit of the specific works area during all phases, the methodology is to be accepted by FA prior to implementation.

Control of the Contractor's Personnel and Security

The directions of the SO shall bind all personnel associated with the work in respect of operational safety matters.

All personnel requiring access to the works site must have appropriate identification identifying them as an employee of the Contractor and have a valid Airport ID card.

Additionally, the following provisions will apply to all Contractor personnel carrying out works on the airside of the airport:

- All workers must attend a security / Safety briefing provided by the Airside & AVSEC Section;
- All vehicles must use only designated access routes;
- All Contractor personnel must comply with any additional security provisions, which may be imposed by FA;
- Unauthorized persons must not enter the Works Area.
- Persons and vehicles shall be subjected to security screening prior to every entry onto Airside.
- Any sites controlled by the Contractor Landside are to be secured by the Contractor's personnel against theft or interference;
- The Contractor is to maintain records of all personnel entering or leaving any site within the Security Restricted Area (this is usually done at the entry gate); and
- Animals and firearms are prohibited airside.

FA reserves the right to limit or restrict access to airside areas at short notice to comply with security systems and/or procedural variations resulting from increases in aviation threat levels.

Employees of The Contractor shall obey any directions given by Safety Officers or FA Authorised Officers.

Access to the Works Area

Specific access routes to and from the works areas will be as directed by the SO (The Contractor is to liaise with the SO to confirm the access routes and update the plan attached in Appendix B).

Movement of vehicles, plant and equipment must be confined to designated routes in order to minimise tracking of dirt and debris onto airfield pavements and to prevent damage to airport lighting.

Parking

Equipment parking must only be located in areas designated to the Contractor by the SO.

Contractor's Vehicles and Plant

Vehicles allowed airside must be checked by the Airside Operation officer before an Airside Vehicle Permit can be issued. The designated drivers will have to show a valid driving licence and a letter from the Contractor to indicate they are designated drivers before they are issued with a temporary AVP.

No movement of vehicles or plant is to take place outside the works areas or designated access routes without the consent of the SO. Only vehicles and plant actually engaged in the work shall be permitted at the works site.

At the end of each work period or in preparation for a landing, all vehicles and plant shall be moved clear of the movement area and parked in an area pre-designated for parking.

Private vehicles belonging to Contractor personnel shall be permitted to park only in the public car park areas or such other areas made available by FA. Private vehicles will not be permitted airside.

All vehicles used airside must be covered under The Contractors full comprehensive insurance, including third party insurance.

All vehicles allowed airside must be equipped with a portable fire extinguisher.

Reinstatement of Disturbed Areas

All natural surface areas that are disturbed by earthworks or the passage of vehicles to/from any airside worksite must be stabilised against erosion and reinstated with turf at the completion of works.

Cleanliness

Measures shall be taken at all times for control of dust, debris or other nuisance materials and the Contractor shall immediately respond to any direction by the SO to eliminate a problem.

The Contractor is to ensure that aircraft pavements used or crossed during the works are kept clean and free of debris at all times. The SO on duty will determine when the pavement is in a sufficiently clean condition to allow the safe operation of aircraft.

The Contractors personnel are to take every precaution to prevent any spillage of material on or in the vicinity of aircraft movement areas, or in transit to and from the work site. Any spillage shall be removed by the Contractor to the satisfaction of the SO.

For airfield pavement areas that are to be returned to operational service following the completion of the work and within the remaining period, the treated pavements shall be completed and cleaned to a condition suitable for aircraft operations. Rolling, brooming, line marking, cleaning and plant removal may then be undertaken until 30 minutes prior to the completion of the stage detailed in the MOWP, at which time the work site shall be vacated.

Prior to the completion of each stage, the Contractor is to:

- Remove all personnel, equipment and rubbish from the work site;
- Ensure all pavements are swept clean and left in a condition assessed as serviceable and safe by the Safety Officer;
- Restore any damaged areas to the satisfaction of the SO; and
- areas where work is being conducted is marked off with cones

The Contractor must employ a fully operational self-propelled vacuum sweeper, or approved equivalent, to clean each work area throughout the duration of each work period.

Visual Ground Aids

The Contractor must protect all runway and apron edge lights in each work area through the installation of approved barriers. The barriers must be placed prior to the commencement of work and must remain in place for the duration of the work period/day that work is being carried out.

Security

The requirements relating to operational safety set out in CAAF Standard Document SD 139-05 shall apply and the directions of the Safety officer shall bind all personnel associated with the work in respect of operational safety matters.

All personnel requiring access to the works site must have valid ID's issued by FA Security and comply with the above as well as any other security requirements provided by the Safety Officer from time to time.

All personnel are to wear their Security passes on the outside of their clothing / PPE at all times while on the airport. At the completion of the Works, all security passes shall be handed back to FA.

Aviation Security officers will be monitoring all works associated with the Project airside for security reasons.

Communications

The Safety Officer will have possession of a portable VHF Air Band transceiver radio tuned to the Control Tower frequency. This radio will be carried at all times by the Safety Officer whilst works are in progress on the movement area. The Safety Officer shall be the primary contact for the Control Tower for all safety related communications and to warn the contractor to vacate the movement area due to aircraft movements.

For general discussions and problems regarding the project, the Control Tower shall contact the FA Project Manager.

5. ADMINISTRATION

Project Manager

This position is held by: Ashley Kumar
Contact Number (s): 9929167

Is responsible for the operational safety aspects of the project. The Project Manager's is to be represented on site by the Safety Officers who will communicate with the Contractors Representative and Contract Administrator on matters necessary for ensuring the safe progress of the work.

Works Safety Officer

Is nominated as being: Duty Officer
Contact Number (s) 9906021

The SO shall be responsible for the safe and effective implementation of project and includes;

- Ensure safety of aircraft operations in accordance with directions in the MOWP during project implementation;
- Ensure that, where applicable, works are notified by issue of a NOTAM;
- Liaise daily with ATC on information pertinent to the safety of aircraft operations;
- Discuss daily with the Project Manager and Contractor's Representative, any matters necessary for the safety of aircraft operations;
- Ensure that unserviceable portions of the movement area, temporary obstructions, and the limits of the works area are correctly marked in accordance with the MOWP;

- Ensure that all other requirements relating to vehicles, plant and equipment and materials are complied with;
- Establish access routes for the Contractor to and from the work areas; and
- Report immediately to ATC, MAO and the GM ATM & OPS any incident, or damage to facilities, likely to affect the safety of aircraft.

Engineer's Representative

Is nominated as being: Janek Czustka - GHD

Contact Number (s): M: +64 272 161 782 (FJ number to be confirmed)

The responsibilities of the Engineer's Representative is as follows:

Ensure that work is carried out in accordance with the MOWP;
 Ensure safety of aircraft operations in accordance with directions in the MOWP during project implementation;
 Ensure that work is carried out in accordance with the Technical Specification;
 Ensure the Contractors personnel follow directions given by the SO.

Contractor's Representative

Is nominated as being: Abdul Shameen Rafiq / Sameet Chand

Contact Number (s): 9994477 / 9991420

The responsibilities of the Contractor's Representative is as follows:

- Ensure that the Contractors personnel involved in the Project are well trained and briefed on safety requirements, the rules and regulations associated within this MOWP.
- Inducted frequently on Health and Safety Requirements and wear the appropriate PPE.
- Ensure that all the contract and related works are carried out carefully so as not to damage existing facilities and services and in accordance with the Contract technical specifications and best practice.
- Ensure that vehicles, plant and equipment involved in the project are licensed, appropriately marked and equipped with flashing orange beacons.
- Ensure that work is carried out in accordance with the recognised standards.
- Ensure the contracted personnel follow directions given by the SO.
- Ensure that the Contractor has implemented the contract management plan.

Distribution List

This MOWP is to be distributed to the persons and organisations listed in Section 8.

6. AUTHORITY

Issue

This MOWP is issued in accordance with the Nadi Aerodrome Manual – Aerodrome Works Safety Section 4.8-1.

All works will be carried out in accordance with the MOWP.

Variation

No variation to this MOWP is to take place without the written approval of the FA Project Manager in consultation with the Contractor.


Expiry

This MOWP will remain in force until the, unless extended by an amendment.

Approval

This MOWP is authorised by FA & all works will be carried in compliance with these requirements and approved by the Executive Chairman of Fiji Airports.

Project Manager 

Contractor's Representative 

Consultant Engineer (GHD) 

Manager Airside Operations: 

7. DRAWINGS

To be used as guide drawings only. Final and as-maintained design drawings

8 DISTRIBUTION LIST

To include:

Fiji Airports, Project Manager, Contractor's Representative, MAO, GM ATM & OPS, Safety Officer, Air Traffic Control, MASS, MARFFS,MSRM, and CAAF.

APPENDIX

'A'

CAAF SD 139-05

***APPENDIX
'B'***

CONSTRUCTION PLANS

***APPENDIX
'C'***

***CONSTRUCTION
PROGRAM***

APPENDIX D:
INSPECTION CHECKLIST


AIRPORT: NADI INTERNATIONAL

DATE: **TIME:**

LOCATION AND BRIEF DESCRIPTION OF WORKS:

ITEM	DESCRIPTION	YES / NO	COMMENTS	N / A
1	Are all Navigation aids, airfield ground lighting and paint markings are in working order, correctly located and clean?			
2	Has all construction materials, equipment and plant been removed from the works area and located a safe distance from the operational pavements and OLS?			
3	Has all soiled pavement surfaces within the works area been thoroughly cleaned via appropriate means, removed from the operational strips and free from FOD?			
4	Has all operational pavement surfaces used as access to the site been thoroughly cleaned and free from FOD?			
5	Has all excavations been appropriately backfilled, or appropriately marked with red/white stakes or cones, for safe aircraft operations?			
6	Has an appropriate NOTAM been issued in accordance with the progress of the works and is a true representation of the present airfield pavement condition?			
7	Are all personnel involved with the works removed from the operational strips?			

The inspection has been completed by:

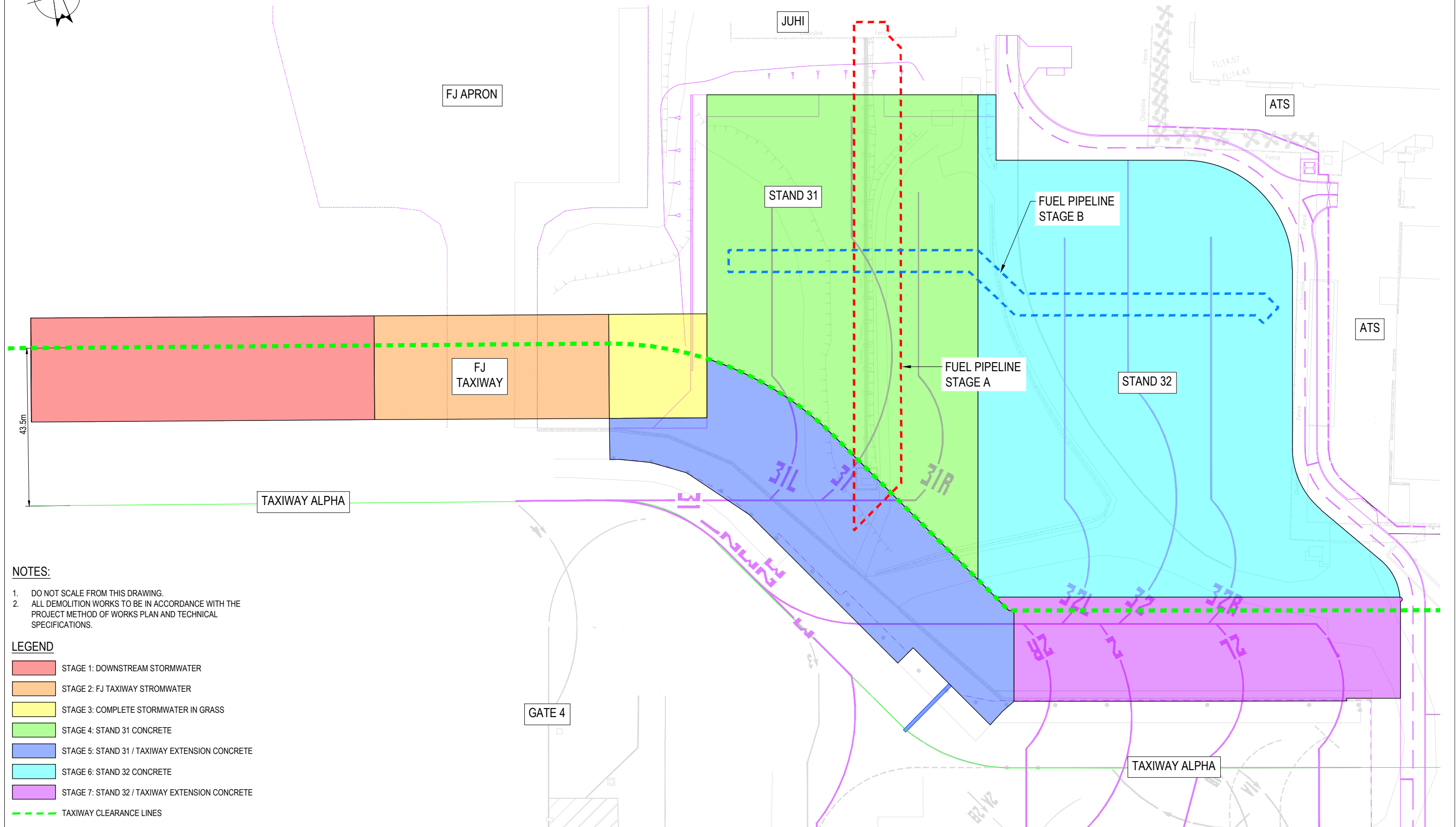
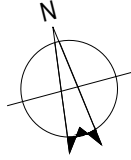
ROLE	NAME	SIGNATURE
CONTRACTORS REPRESENTATIVE	ABDUL SAMIN	
ENGINEERING SUPERVISOR		

The work site and access routes have been cleaned to an acceptable level by the Contractor, the Airfield has been left in a safe and operable condition, has been inspected by the Works Safety Officer and the Airfield can formally be 'handed-over' for full control by Airports Fiji Limited.

SAFETY OFFICER: PRINT NAME: **SIGNED:**

Authorization by:

Project Manager / Designate: **Signed:**



- NOTES:**
- DO NOT SCALE FROM THIS DRAWING.
 - ALL DEMOLITION WORKS TO BE IN ACCORDANCE WITH THE PROJECT METHOD OF WORKS PLAN AND TECHNICAL SPECIFICATIONS.

LEGEND

[Red Box]	STAGE 1: DOWNSTREAM STORMWATER
[Orange Box]	STAGE 2: FJ TAXIWAY STORMWATER
[Yellow Box]	STAGE 3: COMPLETE STORMWATER IN GRASS
[Green Box]	STAGE 4: STAND 31 CONCRETE
[Blue Box]	STAGE 5: STAND 31 / TAXIWAY EXTENSION CONCRETE
[Cyan Box]	STAGE 6: STAND 32 CONCRETE
[Purple Box]	STAGE 7: STAND 32 / TAXIWAY EXTENSION CONCRETE
[Dashed Green Line]	TAXIWAY CLEARANCE LINES

ISSUE FOR CONSTRUCTION