

METHOD OF WORKING PLAN [MOWP]

AERODROME LABASA AIRPORT, LABASA, VANUA LEVU (LBS)

2018/19 LABASA AIRPORT APRON & CARPARK UPGRADE, APRON EXTENSION AND RUNWAY UPGRADE

PROJECT DESCRIPTION Rehabilitation of the existing apron; Extension of the apron by approximately 8m southwest to suit the new terminal location and to enable the tail of an ATR72 aircraft to clear the OLS; Taxiway surface improvement; Car Park surface improvement and extension, Realign access into the airport, associated drainage required for apron, Runway pavement upgrade and car park area and Interfacing of taxiway and apron lighting design provided by Fiji Airports.

REFERENCE NUMBER FA/MOWP/18/Labasa Apron Construction and Runway Upgrade

DATES

Approval of MOWP: 2020

Commencement of Works: 18th June 2020

Completion of Work: 9th September 2020

Expiry of MOWP: 2020

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1. WORKS INFORMATION

1.1 Background

Fiji Airport (FA) has proposed to upgrade the existing Apron, Runway and Car park pavement which has deteriorated over the past years at Labasa Airport. This involves the structural upgrade of the existing pavement on the apron, Runway and carpark, apron extension, taxiway improvement, construction of concrete stand pads for Code C Turbo- Prop aircraft, new paint markings and relocation of the entry/exit road referencing the new terminal building location.

This Method of Works Plan is concerned with the construction stage of the project that involves Grade 2/4 chip seal with prime and bitumen emulsion sand locking seal for Runway, Apron and Taxiway and Grade 3/5 chip seal for Car Park. The construction stage works will also include works on storm water network upgrade for apron and car park, paint marking, new entry culvert construction, concrete stand pad construction and incorporation of AGL works for Taxiway and Apron.

The construction staging plans for the works is attached in **Appendix B**.

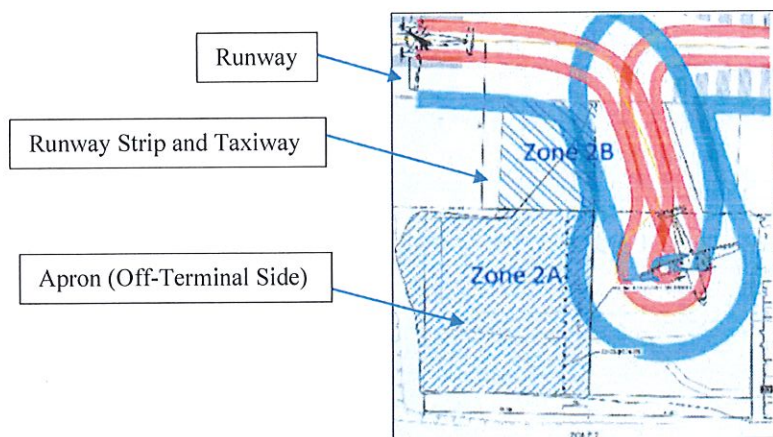
1.2 Description of Work

1.2.1 Apron

This phase of work involves removal and disposal of the existing top 450mm of seal surface and Pavement Layer.

The excavated material from the existing pavement layer will be reused as structural fill for the apron extension area. The exposed layer will be compacted and replaced with new AP 65 subbase and Ap 40 basecourse Layers to required levels. Cement stabilize of the top 200 mm Ap 40 base course layer lightly to aid against structural deformation, prime coat the finished base course surface and bitumen seal with two coat chip seal G2/G5 and lock in with a bitumen emulsion sand seal. The work also includes removal of existing lead In/Out lines and temporary marking of Lead-in and Lead-out lines for the two work zones. Upon completion of construction works, permanent marking of taxiway and apron paint markings. The work area is divided into work zones to maximize available working period, working space and to avoid disruption to scheduled aircraft movement.

1.2.1.1 Work Zone 2 (Western side of Apron) – 18th June to 24th July 2020

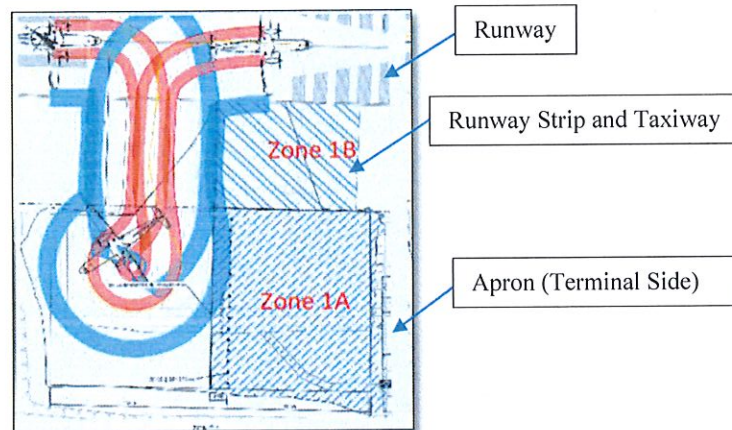


- Construct temporary paint markings on work zone 2
- Remove temporary paint markings on work Zone 1
- Mark off work areas and barricade Work zone 2 as per construction/staging drawing
- Ensure safe movements of passengers and luggage
- Excavate and remove existing sealed surface and base course material from taxiway and apron in Work zone 2
- Lay and compact excavated material as structural fill for apron extension area for Work zone 2
- Fill and compact AP65 sub-base course as required on the apron extension area

- Fill and compact 200 mm thick AP40 base course material to the design levels for the taxiway, apron and apron extension area in Zone 2
- Cement stabilize new base course material and construction of bitumen seal with two coat chip seal grade 2/4 and lock in with a bitumen emulsion sand seal
- Construction of Drainage network for the apron as per construction drawing
- Remove temporary paint markings on Work zone 1
- Construct permanent paint markings for the entire apron and taxiway
- Lay and compact 150mm of top soil on the batters and seed grass. Top soil is to be free of all foreign materials, rocks, stones and cobbles

1.2.1.2 Work Zone 1 (Eastern Side of Apron) – 28th July to 7th September 2020

Work Zone 1 is divided into 2 sections, Zone 1A and Zone 1B.



- Remove existing lead in, lead out lines and other paint markings on Work zone 1, Work zone 2.
- Construct temporary paint markings on Work zone 2.
- Mark off work areas with cones and barricade Work zone 1 as per construction/staging drawing
- Ensure safe movements of passengers and luggage
- Excavate and remove existing sealed surface and pavement material from taxiway and apron in Work zone 1
- Lay and compact excavated material as structural fill for apron extension area for Work zone 1
- Fill and compact AP65 sub-base course as required on the apron extension area
- Fill and compact 200 mm thick AP40 base course material to the design levels for the taxiway, apron and apron extension area in Zone 1
- Cement stabilize new base course material and construction of bitumen seal with two coat chip seal (19/10) and lock in with a bitumen emulsion sand seal
- Construct concrete stand pads and cure for the required period.
- Construction of Drainage network for the apron as per construction drawing
- Lay and compact 150mm of top soil on the batters and seed grass. Top soil is to be free of all foreign materials, rocks, stones and cobbles.

1.2.2 Runway Upgrade (CH00-CH1005.7) – 13th July to 4th August 2020

This phase of work involves removal and disposal of the existing seal surface from Ch165.7-Ch665.7 along the center of the runway (5m on either side of the centerline) which includes excavation of top 300 mm layer of the existing seal and pavement layer. The total area of excavation is 5000m². Compaction of exposed layer with proof rolling is required. It will be replaced with a new AP 40(M4) basecourse layer to required levels. Cement stabilize of the top 200 mm base course layer lightly to aid against structural deformation. Once the testing is completed and approval is given by the engineer to proceed, prime coat will be applied to the base course surface. The primed section will be left for 12 hours for curing. After curing process, a 2 coat chip seal grade 2/4 will be applied to the digout section and reseal will be carried from Ch00 to Ch165.7 and Ch665.7 to Ch1005.7. Upon completion of sealing works, permanent marking of centerline will be carried out. The entire construction works will be completed during 3 weeks shutdown period.

1.2.3 Car Park – TBA

This phase of work involves removal of trees, excavation part of the existing access road seal surface and pavement layer. Installation of twin 900mm RRJRC pipe including the in-situ headwall under the new access road and 450dia RRJRC pipes and catch pits on the car park as per the design. The exposed subgrade layer will be compacted and tested. The pavement layer will be strengthened by placing a 250mm new AP65 subbase layer. After the test is completed on the subbase layer and approval is given to proceed, Kerb and Channel will be constructed as per the design. Also Placement of 200mm AP 40 basecourse layer will be carried simultaneously which will include compaction and final trimming. Once base works are completed and tested and approved by the engineer. A prime coat will be applied to surface followed by two coat chip seal (Grade 3/5). All the utility services will be installed during the construction as per the design and specification requirements. Upon completion of entire sealing works line marking and signage installation will be completed as per the design. Appropriate Traffic Management will be in place to execute the above works.

1.2.4 Construction Traffic

It is proposed that construction traffic will enter and exit the airside works areas via Terminal Side Apron Access Gate nominated construction access route as marked on the construction drawing.

All access to the worksites by the Contractor's personnel, plant and equipment will be under strict Safety Officer (SO) escort.

Specific access routes to and from the works area will be directed by the Safety Officer. Movement of vehicles, plant and equipment must be confined to these routes in order to minimize tracking of dirt and debris onto aircraft movement area pavements and to prevent damage to airport lighting.

The Civil Contractor is required to keep all pavements used or traversed during the works clean to the satisfaction of the SO. The Contractor shall use mechanical sweepers or suction trucks for the purpose of keeping the pavement surfaces clean and FOD free.

In the event any of the contractor vehicles damaging any facility, pavement, perimeter road or the environment during the work, the Contractor shall inform the Safety Office and replace or repair the damage immediately.

1.3 Commencement Date and Execution of Works

1.3.1 Timing

A NOTAM will be in place for all works. The actual date and time of commencement will be advised by NOTAM, and is to be issued by FA 48 hours before work commences or as soon as possible when the 48 hours' notice cannot be achieved.

The Contractor shall strictly give two clear working days' notice (Monday - Saturday) prior to commencement of works.

Works will be arranged so as not to disrupt scheduled aircraft movements and emergency flights.

However works shall cease whenever directed by the SO

1.3.2 Working Hours

Hours of work for airside and land side works will be as follows (based on current airline schedules, may change as schedules alter)

- Zone 1A : Monday to Saturday : 06:00 to 19:00
- Work 1B: Monday to Saturday : 19:00 to 05:30
- Zone 2A : Monday to Saturday : 06:00 to 19:00
- Zone 2B : Monday to Saturday : 19:00 to 05:30
- Runway : Monday to Saturday : Full Shut Down Period
- Car park : Monday to Saturday : 06:00 TO 19:00

The Time displayed in LOCAL TIME.

1.3.4 Programme of Works

The following duration of works are anticipated: 32 weeks

Mobilization period: 8 weeks

Civil Works Airside: 12 weeks

Civil Works Landside: 12 weeks

1.3.5 Sequence of Work

Apron and Taxiway

The Existing Apron and taxiway surface requires excavation of the sealed surface and overlay with imported AP40 to required levels with the top 200mm layer being cement stabilized. These are significant works thus aircraft operations will need to be restricted to a reduced apron areas.

The Works will be sequenced in to the followed stages

- **Stage 2A: 18th June to 24th July 2020.**
Closure of Western Side of the Apron to aircraft operations. Works during the day will occur on the apron. ATR Wingtip clearances will impact the worksite thus an object free pullback will be necessary during aircraft movements.
- **Stage 2B: 18th June to 24th July 2020.**
Closure of the Western side of the Apron and Taxiway to aircraft operations. Works during the night will occur on the taxiway in the runway strip (Work Zone 2B). No aircraft movement will be permitted at this time.
- **Stage 1A: 28th July to 7th September 2020**
Closure of the Eastern Side of the Apron to aircraft operations. Works during the day will occur on the apron. ATR Wingtip clearances will impact the worksite thus an object free pullback will be necessary during aircraft movements.
- **Stage 1B: 28th July to 7th September 2020**
Closure of the South Western Side of the Apron to aircraft operations. Works during the night will occur on the taxiway in the runway strip (Work Zone 1B). No aircraft movement will be permitted at this time.

Please refer operational staging plans for layout of the works zones and operational closures.

To ensure maximum use of the allocated time slots on the apron and taxiway, the Contractor shall arrange with the Pavement Engineer of FA during the day at least before 6am daily to mark the extent of the area earmarked for construction works.

Confirmation for works to proceed shall be jointly agreed between the Contactor and FA before 6am or prior to starting works subject to weather condition on the day.

All Contractor personnel and vehicles shall be ready to commence entry onto airside at least 2 hours prior to actual commencement of work as NOTAM issued.

All vehicles after clearing security screening shall hold on the entry gate with all personnel remaining in their vehicles.

Once approval is given by SO or PAAE of FA to proceed with the work, only the machines and equipment required for the initial work shall be permitted to enter the maneuvering area whilst the others remain outside entry gate.

At no time shall unnecessary construction vehicles be permitted onto the apron and runway unless specifically authorized by the SO.

Car Park and New Access

The existing car park surface requires excavation of sealed surface and overlay with imported AP65 for subbase and AP40 for base course to required level. The car park is also extended south wards. The car park surface will be sealed with Grade 3/5 chip seal with total works area estimated to be 2730m². The work also includes construction of car park paint markings.

To ensure minimum disruption to airport users, the work area is divided into two work zones and the Contractor is to finish work on one work zone prior to proceeding to the next.

All carpark construction works shall be stopped 30 minutes prior to scheduled flight arrival and start 30 minutes after the flight departure FOD measures should be in place to avoid dust in terminal building and particles onto the runway.

The work also includes construction of a 6 meter wide new access road in align to the new terminal building. The access road works includes excavation of top spoil, construction of sub-base course and base course layers with imported AP65 and AP40 to required level, culvert crossing for Sausaunimeke creek and surface sealed with Grade 3/5 chip seal. The total works area is estimated to be 448m².

The contractor shall implement traffic management within the car park at all times during construction.

2. SAFETY

2.1 Method of Working Plan

Works shall be done in strict accordance with this Method of Working Plan (MOWP)

2.2 Safety of Aircraft Operations at All Times

Aircraft operations and aviation safety take precedence at all times.

The Contractor shall ensure that the Taxiway and apron is returned to service within 90mins of being notified by the SO to stop all work for whatever reason.

2.3 Daily Prior Works Notification.

The Safety Officer (SO) shall contact the tower for mobilization of works before commencement.

3. RESTRICTIONS TO AIRCRAFT OPERATIONS

The following restrictions are anticipated for Aircraft operations:

- Stage 1A: During the construction of the apron Stage 1A, only one aircraft would be accommodated on a sealed surface of Apron at a particular time. Overnight aircraft parking shall not be allowed unless emergency situations prevail
- Stage 1B: Airport CLSD during working times set out in Section 1.3.2.

- Stage 2A: During the construction of the apron Stage 1A, only one aircraft would be accommodated on a sealed surface of Apron at a particular time. Overnight aircraft parking shall not be allowed unless emergency situations prevail
- Stage 2B: Airport CLSD during working times set out in Section 1.3.2.
- FA will publish aeronautical Information pertaining to the Labasa aerodrome operating status via NOTAM's. Any variations to that advised above will be notified further via NOTAM.

4. RESTRICTION TO THE WORKS

4.1 General

The Contractor shall comply with the requirements of CAAF Standard Document, this MOWP and the provision of the specifications for these works (Refer Appendix A).

The Contractor's site representative shall contact the SO at the start of each working shift to ascertain the status for the proposed work with respect to the operational requirements of the aerodrome.

All plant, equipment and materials shall be secured at all times during the work (on site and at laydown area) so that it is not be able to be wind borne. Plant and materials will be stored in such a manner to ensure that they do not infringe the OLS Transition layer of the runway.

All lightings erected and operated by the Contractor shall comply with the Civil Aviation Authority Fiji requirements.

All vehicles and plant operating on or near aircraft operational areas shall display a flashing orange light when vehicles are moving to and from the areas of work.

4.2 Driving

A speed limit of 50kph applies to all vehicles on the general airport movement area and 10kph when within 15 meters of aircraft. Breaches of these speed limits will result in refusal of access to the airside for the driver involved for the remainder of the works.

All vehicle drivers requiring access onto airside shall undertake safety awareness training delivered by FA and shall be escorted at all times by a SO whilst on the maneuvering area.

Any non-compliance with respect to this plan shall see immediate removal of the truck and driver from the project. It is mandatory that all vehicles operating on the airside shall have third party and public liability insurance cover of not less than \$1M FJD.

Drivers must ensure that trucks are correctly loaded and where necessary covered prior to entering and leaving site to prevent any spillage. In the event of spillage the truck driver must immediately notify the project manager / safety officer who will oversee the cleaning of spilled materials by the Contractor.

All vehicular movements on airside shall be confined to pave or gravel surfaces unless authorized by the SO.

4.3 Pavement Cleanliness – FOD Protection

The Contractor is required to keep all aircraft pavements used or crossed during the works clean to the satisfaction of the SO.

No motor vehicle shall leave or enter the site laden with any materials unless the vehicle is loaded and/or covered in a manner that shall prevent the discharge or dropping of any materials.

The Contractor shall ensure that the wheels, tracks and body of all construction plant leaving the site are free from dirt or any other material, which may discharge or drop. Tracked plant or equipment shall not be used on any existing or new pavement surfaces.

Vehicles or plant not in use shall be parked and immobilized outside the runway strip and Apron area or as nominated by the SO.

If the Contractor fails to keep these areas clean, the WSO may arrange cleaning and the cost thereof shall be a debt due and payable by the Contractor to FA

All damages to the airside access road by the contractor shall be repaired to its original status by the Contractor.

4.4 Airport Security and Airside Safety Requirements

Every person desiring to enter the Site shall comply with all safety and security regulations relating to site and with all conditions relating to entry to and behavior on the Site as contained in the Labasa Aerodrome Manual.

4.5 Marking the Unserviceable Area

Markers will comprise white reflective banded orange cones or white banded red marker boards to define the limits of the available movement area (if required).

4.6 Emergencies and Adverse Weather

Under certain emergency situations or in the event of adverse weather conditions, the Contractor may be denied access to the airside of the airport and to the work areas. This will only apply prior to the scheduled commencement of the daily works.

Once closed, the closed portion of the apron will only be reopened for emergency flights after 90 minutes prior notice or on the onset of adverse weather.

4.7 After Completion of Work

Prior to operational handover, the pavements will be inspected and accepted for operational use and safety.

5. NOTAM

The project Works will be planned accordingly to minimize restrictions to schedule aircraft movements. The SO in consultation with the Project Manager and AFISO shall be responsible for initiating NOTAMs relating to the Project and in accordance with paragraph 1.3 above. The NOTAM shall clearly define the dates and times when works will be in progress, what obstacles will be involved and procedures for clearing these obstacles prior to aircraft movement.

5.1 Denial of Access to the Works

During declared airport emergencies and/or poor visibility conditions, the Contractor may be either be refused entry to the Site or removed from the airside. Due to the unforeseen nature of these conditions, it may not be possible to give prior warning to the Contractor. In such circumstances, the Contractor shall advise the Contract Administrator on a daily basis of any claim for cost or extensions of time.

5.2 Inspection Checklist & Operational Sign Off

An inspection checklist will be used during the works to ensure the airfield is left in a suitable condition for operational activities. An example checklist is provided in Appendix B.

5.3 Emergency and Adverse Weather

In case of aircraft emergencies, the Contractor will comply with all SO Instructions for ceasing operations and removing plant and personnel from the immediate location.

In extreme adverse weather, the SO has the authority to stop the work where personnel or operational safety is considered at risk. Work will resume when those conditions abate but at the discretion of the FA.

In such an emergency situation the Contractor shall, if time permits, reinstate the works to a standard required at the close of each shift as follows:

All Materials, Vehicles, Personnel and Equipment shall be removed from the worksite as required under the operational requirements in the specification. This shall be as a minimum that no plant or materials shall intrude into any transitional surface.

Prior to opening, the runway shall be swept and left clean of any dirt or loose debris that may cover markings, or reduce the smoothness or skid resistance of the surface or may be sucked into engines or be thrown up by their prop wash or air blast.

All surfaces shall be reinstated to safe condition for their use.

Complete an inspection of the site with the SO in accordance with the Inspection Checklist as provided in Appendix B.-

Works will be confined only to the work areas on the runway.

FA will provide a Safety Officer (SO) who will have complete authority to direct the Contractor on Aerodrome Operational Requirements.

All staff involved in the works must undergo a site specific briefing in relation to works safety and security requirements prior to commencing on site. FA will deliver the first briefing and thereafter the Contractor will be responsible for briefing new workers on safety and security requirements.

The Contractor shall comply with the requirements of the Contract Documents produced for this project and this MOWP. The Contractor's site representative shall contact the SO at least one hour prior to the start of each working period to ascertain the status for the proposed work with respect to the operational requirements of the aerodrome.

Prior to commencing each works period, the Contractor personnel shall meet with the SO and the Project Manager or designate in order to ensure that all parties including sub-contractors are aware of the working requirements and the work restrictions for each stage.

Smoking is not permitted on airside, or within any FA Airport buildings. A designated smoking area can be designated by FA outside the perimeter fence when requested to by the Contractor.

Upon completion of each work period and before each aircraft operation, the works areas are to be made serviceable to the satisfaction of the SO.

5.4 Personnel, Equipment, Plant and Materials

The nominated personnel for key roles are specified in Section 5: Administration.

All personnel involved on this job shall be bound by any instructions issued by the Safety Officer either verbally and/or written. The Safety Officer may refuse access to persons likely, in his opinion, to compromise aircraft safety on the aerodrome.

Personnel are to be confined to the defined work area and access routes at all times.

All staff are to wear high visibility jackets and safety boots or safety shoes whilst working on the airport. Staff must also wear hand gloves, dust protectors, safety glasses and ear muffs (as required through Contractors Health and Safety requirements).

The Contractors access will be limited to the work areas as shown on the Access Layout plan in Appendix B.

All construction work will be required to withdraw personnel and equipment from the construction area in the event of an emergency.

Only equipment, plant and materials that are required for daily construction activities shall be permitted Airside.

5.5 Aircraft Operators

Aircraft Operators and Airlines are required to notify FA at least 24 hours prior to any proposed deviation to the approved flight schedules. Aircraft operators who do not notify changes in a timely manner will be required to re-schedule or incur significant delays.

5.6 Works Limits

All works and the Contractors Constructor's plant, equipment, materials and personnel shall be confined to the areas established by the Contractor, in coordination with the SO. White banded orange cones will define the limit of the specific works area during all phases.

5.7 Control of the Contractor's Personnel and Security

The directions of the SO shall bind all personnel associated with the work in respect of operational safety matters.

All personnel requiring access to the works site must have appropriate identification identifying them as an employee of the Contractor and have a valid Airport ID card.

Additionally, the following provisions will apply to all Contractor personnel carrying out works on the airside of the airport:

- All workers must attend a security / Safety briefing provided by the Airside & AVSEC Section;
- All vehicles must use only designated access routes;
- All Contractor personnel must comply with any additional security provisions, which may be imposed by FA;
- Unauthorized persons must not enter the Works Area or airside.
- Persons and vehicles shall be subjected to security screening prior to every entry onto Airside.
- Any sites controlled by the Contractor Landside are to be secured by the Contractor's personnel against theft or interference;
- The Contractor is to maintain records of all personnel entering or leaving any site within the security Restricted Area (this is usually done at the entry gate);
- Animals and firearms are prohibited airside; and
- Food consumption and smoking are prohibited on airside.

FA reserves the right to limit or restrict access to airside areas at short notice to comply with security systems and/or procedural variations resulting from increases in aviation threat levels.

Employees of the Contractor shall obey any directions given by Safety Officers or FA Authorized Officers.

5.8 Access to the Works Area

Specific access routes to and from the works areas will be as directed by the SO (The Contractor is to liaise with the SO to confirm the access routes and update the plan attached in Appendix B).

Movement of vehicles, plant and equipment must be confined to designated routes in order to minimize tracking of dirt and debris onto airfield pavements and to prevent damage to airport lighting.

5.9 Parking

Equipment parking must only be located in areas designated to the Contractor by the SO.

5.10 Contractor's Vehicles and Plant

Vehicles allowed airside must be checked by the Airside Operation officer or designate before an Airside Vehicle Permit can be issued. The designated drivers will have to show a valid driving license and a letter from the Contractor to indicate they are designated drivers before they are issued with a temporary AVP.

No movement of vehicles or plant is to take place outside the works areas or designated access routes without the consent of the SO. Only vehicles and plant actually engaged in the work shall be permitted at the works site.

At the end of each work period or in preparation for a landing, all vehicles and plant shall be moved clear of the movement area and parked in an area pre-designated for parking.

Private vehicles belonging to Contractor personnel shall be permitted to park only in the public car park areas or such other areas made available by FA. Private vehicles will not be permitted airside.

All vehicles used airside must be covered under The Contractors full comprehensive insurance, including third party insurance.

All heavy vehicles allowed airside must be equipped with a portable fire extinguisher.

5.11 Reinstatement of Disturbed Areas

All natural surface areas that are disturbed by earthworks or the passage of vehicles to/from any airside worksite must be stabilized against erosion and reinstated with turf at the completion of works.

5.12 Cleanliness

Measures shall be taken at all times for control of dust, debris or other nuisance materials and the Contractor shall immediately respond to any direction by the SO to eliminate a problem.

The Contractor is to ensure that aircraft pavements used or crossed during the works are kept clean and free of debris at all times. The SO on duty will determine when the pavement is in a sufficiently clean condition to allow the safe operation of aircraft.

The Contractors personnel are to take every precaution to prevent any spillage of material on or in the vicinity of aircraft movement areas, or in transit to and from the work site. Any spillage shall be removed by the Contractor to the satisfaction of the SO.

For airfield pavement areas that are to be returned to operational service following the completion of the work and within the remaining period, the treated pavements shall be completed and cleaned to a condition suitable for aircraft operations. Rolling, brooming, line marking, cleaning and plant removal may then be undertaken until 30 minutes prior to the completion of the stage detailed in the MOWP, at which time the work site shall be vacated.

Prior to the completion of each stage, the Contractor is to:

- Remove all personnel, equipment and rubbish from the work site;
- Ensure all pavements are swept clean and left in a condition assessed as serviceable and safe by the Safety Officer;
- Restore any damaged areas to the satisfaction of the SO; and
- Areas where work is being conducted is marked off with cones

The Contractor must employ a fully operational self-propelled vacuum sweeper, or approved equivalent, to clean each work area throughout the duration of each work period.

At the completion of each work stage, FA representatives / SO will undertake a commissioning inspection.

5.13 Visual Ground Aids

The Contractor must protect all runway and apron edge lights in each work area through the installation of approved barriers. All underground cables must be marked and communicated accordingly to the contractor for their awareness prior to the start of work. The barriers must be placed prior to the commencement of work and must remain in place for the duration of the work period/day that work is being carried out.

5.14 Security

The requirements relating to operational safety set out in CAAF Standard Document SD 139-05 shall apply and the directions of the Safety officer shall bind all personnel associated with the work in respect of operational safety matters.

All personnel requiring access to the works site must have valid ID's issued by FA Security and comply with the above as well as any other security requirements provided by the Safety Officer from time to time.

All personnel are to wear their Security passes on the outside of their clothing / PPE at all times while on the airport. At the completion of the Works, all security passes shall be handed back to FA.

The designated Aviation Security officers will be monitoring all works associated with the Project airside for security reasons.

5.15 Communications

The Safety Officer will have possession of a portable VHF Air Band transceiver radio tuned to the Control Tower frequency. This radio will be carried at all times by the Safety Officer whilst works are in progress on the movement area. The Safety Officer shall be the primary contact for the Control Tower for all safety related communications and to warn the contractor to vacate the movement area due to aircraft movements.

For general discussions and problems regarding the project, the Control Tower shall contact the FA Project Manager.

6. ADMINISTRATION

Project Manager

This position is held by: Ashley Kumar
Contact Number (s): 9929167

Is responsible for the operational safety aspects of the project. The Project Manager's is to be represented on site by the Safety Officers who will communicate with the Contractors Representative and Contract Administrator on matters necessary for ensuring the safe progress of the work.

Safety Officer

Is nominated as being: Duty Officer – Chandra
Prem
Contact Number (s) 9906183

The SO shall be responsible for the safe and effective implementation of project and includes;

- Ensure safety of aircraft operations in accordance with directions in the MOWP during project implementation;
- Ensure that, where applicable, works are notified by issue of a NOTAM;
- Liaise daily with AFISO on information pertinent to the safety of aircraft operations;
- Discuss daily with the Project Manager and Contractor's Representative, any matters necessary for the safety of aircraft operations;
- Ensure that unserviceable portions of the movement area, temporary obstructions, and the limits of the works area are correctly marked in accordance with the MOWP;
- Ensure that all other requirements relating to vehicles, plant and equipment and materials are complied with;
- Establish access routes for the Contractor to and from the work areas; and
- Report immediately to AFIS officer on duty, OMOIA and the GM ATM & OPS on any incident, or damage to facilities, likely to affect the safety of aircraft.
- The Safety Officer shall provide weekly Safety Brief Report to MSRM, OMOIA, Project Manager and GMATM & OPS on the work progress and any safety concerns together with the recommendations.

Engineer To Contract

Is nominated as being: Number (s): 8952572

The responsibilities of the Engineer's Representative is as follows:

- Ensure that work is carried out in accordance with the requirements of this document;
- Ensure safety of aircraft operations in accordance with directions in this MOWP during project Implementation
- Ensure that work is carried out in accordance with the Technical Specification;
- Ensure the Contractors personnel follow directions given by the SO.

Contractor's Representative

Is nominated as being: Darmesh D. Reddy

Contact Number (s): +679 9985742

The responsibilities of the Contractor's Representative is as follows:

- Ensure that the Contractors personnel involved in the Project are well trained and briefed on safety requirements, the rules and regulations associated within this MOWP.
- Inducted frequently on Health and Safety Requirements and wear the appropriate PPE.
- Ensure that all the contract and related works are carried out carefully so as not to damage existing facilities and services and in accordance with the Contract technical specifications and best practice.
- Ensure that vehicles, plant and equipment involved in the project are licensed, appropriately marked and equipped with flashing orange beacons.
- Ensure that work is carried out in accordance with the recognized standards.
- Ensure the contracted personnel follow directions given by the SO.

Distribution List

This MOWP is to be distributed to the persons and organizations listed in Section 9.

7. AUTHORITY

Issue

This MOWP is issued in accordance with the Aerodrome Manual – Aerodrome Works Safety Section 4.8-1.

All works will be carried out in accordance with the MOWP.

Variation

No variation to this MOWP is to take place without the written approval of the FA Project Manager in consultation with the Contractor.

Expiry

This MOWP will remain in force until the, unless extended by an amendment.

Approval

This MOWP is authorized by FA & all works will be carried in compliance with these requirements and approved by the Executive Chairman of Fiji Airports.

Project Manager

 04-06-2020

Contractor's Representative

 04/06/20

Engineer Rep. to Contract

 04/06/2020

8. DRAWINGS

To be used as guide drawings only. Final and as-maintained design drawings

9. DISTRIBUTION LIST

To include:

Fiji Airports, Project Manager, Contractor's Representative, MAO, GM ATM & OPS, Safety Officer, Air Traffic Control, OMOIA, MASS, MARFFS, MSRM, and CAAF.

APPENDIX

'A'

APPENDIX

'B'

Construction Plans

APPENDIX

'C'

Inspection Checklist